



INTEGRATING ART, ENGINEERING, AND THE ENVIRONMENT

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Business Park Planning & Design Charrette: Conclusions, Recommendations and Framework Plan February, 2010



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INTRODUCTION

In the fall of 2009, Harmony Design and Engineering was contracted by the City of McCall, Idaho to facilitate a design charrette and formulate a conceptual plan for a potential Business Park development in the City's Area of Impact. This report is a comprehensive summary of the Business Park Planning and Design Charrette which took place September 29th through October 1st, 2009. This report outlines the events leading up to the charrette, the public meetings and workshops, and the post charrette development of a final Framework Plan and recommendations for Business Park design and development standards.

BACKGROUND INFORMATION

CITY OF McCALL

The history of McCall, Idaho is entwined in the ebb and flow of four overlapping elements: recreation, mining, forestry and commerce. Over time, mining and forestry activities have declined, but recreation and tourism have continued to be the base of McCall's economy. With the opening of Tamarack Resort in 2004, McCall was again "discovered" as a desirable resort destination which resulted in associated development and business growth, particularly in construction.

However, as the nation has suffered serious economic setbacks, McCall also has been affected. The recent closure of Tamarack Resort and other declining economic trends has resulted in an 19.3% unemployment rate in Valley County (December, 2009, Department of Labor).

Regardless, peaks and valleys in the economic health of McCall are nothing new, and the residents continue to be resourceful and innovative in their efforts to maintain a vital, thriving community. They have demonstrated openness to new opportunities, as evidenced by public participation in City-led planning activities.

EVOLUTION OF THE BUSINESS PARK CONCEPT

The City of McCall updated the McCall Area Comprehensive Plan (December 2007), using an intensive community involvement process over an 18 month period. As part of the planning process, property owners, developers, business owners, residents, economic development specialists and planning professionals analyzed past economic

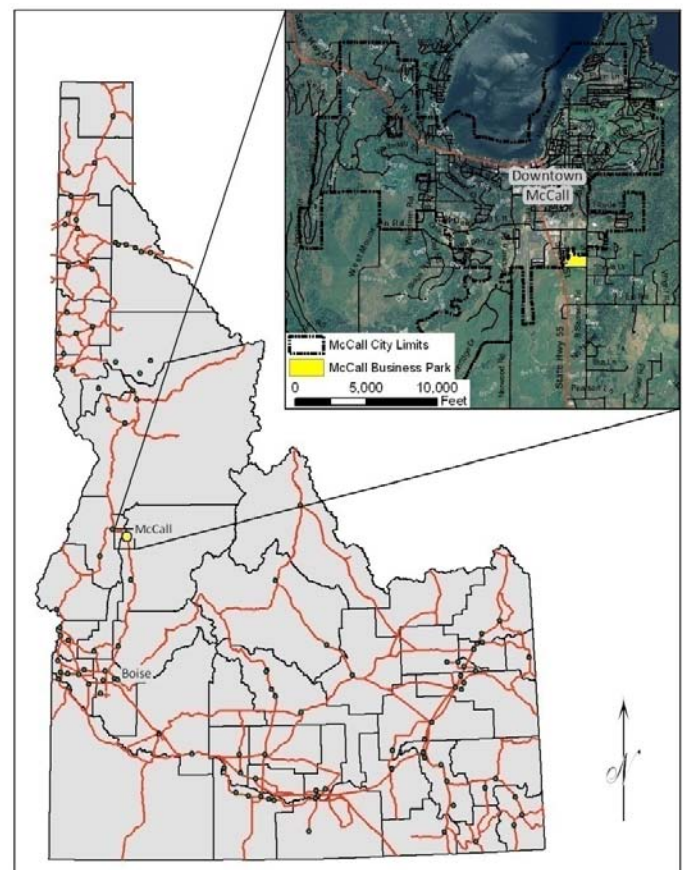


Figure 1: City of McCall Vicinity Map

trends and current conditions. The purpose of this was to assess McCall's strengths and weaknesses, and determine strategies to improve McCall's economy.

This economic analysis provided the following findings:

- **McCall is part of the larger Valley County region and has a strong economic link to the Treasure Valley area market (Figure 1).**
- **The area's economy has transitioned from a resource-based economy to a visitor-based economy, which creates a substantial "peak and valley" pattern in economic activity.**
- **A key factor in influencing future growth in McCall is the evolution of a diversified economy (regional services, manufacturing, etc.).**

These findings, among others, helped the planning participants to determine strategies to address future economic growth in McCall. Discussion of economic diversification strategies yielded a number of objectives that have been included in the Comprehensive Plan. Specifically, one such objective is to "encourage *diversification of the economy* by providing zoned and serviced land for Business Parks and light industry".

Through the economic analysis, it was determined there is not sufficient land zoned for commercial or industrial uses within the City limits and the Impact Area. Within McCall, 11% of the land is currently zoned either Community Commercial or Industrial. In the Area of Impact, only 3.5% of the land has either designation.

The "Business Park" zoning designation has not yet been written for inclusion in the McCall City Zoning Code, but staff and property owners see a pressing need to create such a zoning designation in order to foster economic growth as outlined in the Comprehensive Plan. During the comprehensive planning process, several areas in the area were identified as *potential* Business Park use on the Future Land Use Plan map. One of the areas was 37 acres of land near Krahn Lane/Samson Trail.

In April 2008, the McCall City Council held a work session with City engineers, planning staff, and property owners along Krahn Lane to discuss the potential of annexation and future Business Park development on the Krahn Lane/Samson Trail land. The process for annexation was reviewed, as well as an analysis of the necessary water, sewer, stormwater and road improvements that would be associated with annexation and development. The Council felt it was necessary for this area to have an area plan before annexation, created in concert with landowners and adjacent residential neighbors, to create a concept for road systems, other infrastructure needs, development standards and appropriate land uses.

It is expected that the results of this project will foster economic growth in the McCall area by creating zoning standards that will allow Business Park development as well as specifically designated land in which diverse businesses and industries can be established. Much of the land in question is vacant because properties are zoned Commercial or Industrial but do not have the infrastructure in place to support these types of businesses. If this land is not annexed and supplied with proper infrastructure for commercial development, the land will likely be lost to other uses that do not promote job creation. Several property owners from the Krahn Lane/Samson Trail area have expressed interest in annexing their property and

improving infrastructure. The design charrette process is intended to ensure that the land owners are vested in the concept plan and that the plan is compatible with the community's goals. Creating an area plan and a new Business Park zoning code is expected to provide the opportunity for the establishment of new businesses and jobs in McCall.

PURPOSE OF THE DESIGN CHARRETTE

In order to develop a comprehensive area plan for the Krahn Lane / Samson Trail land, it was decided to hold a "design charrette", which is an intensive planning exercise that involves residents, property owners, business owners, economic development specialists, environmental groups, service providers, utility companies, other community stakeholders, as well as planning staff, public works staff, engineers, and consultants. During two day-long sessions, these stakeholder groups gathered in targeted sessions to discuss and design a land use plan for the 37-acre Krahn Lane Business Park area. All sessions were open to the general public.

In addition, the information from the charrette participants will be used to develop design standards and zoning code recommendations for the designation of a Business Park Zone into the McCall City Zoning Code (Title 3). Ultimately, property owners who wish to change the designated use of their land will have the opportunity to apply for a zone change to Business Park Zone, thereby increasing the amount of land available for future business development in the McCall area.

STUDY AREA / EXISTING CONDITIONS

The 37-acre Krahn Lane Business Park study area is located south of McCall adjacent to the city limits. The property is bordered on the south by Krahn Lane and to the east by Samson Trail. The area lies within the City of McCall Area of Impact, and currently has two zoning designations: 29.2 acres are zoned Community Commercial and 7.5 acres are zoned Industrial.

The Krahn Lane Business Park area has 21 different land owners and 24 parcels (Figure 2). Coordination and support among the property owners is integral to creating a successful Business Park plan in this area. Current uses in the area include an RV campground, landscaping companies, a construction company, an auto collision repair business, residential homes, and vacant land. Two property owners currently have development plans for storage units in the southeast portion of the study area.

The terrain is slightly rolling and there is a 40-foot change of elevation between the highest point and the lowest point of the property (Figure 2). At present, private driveways access perimeter properties and Samson Court accesses 6 parcels in the northeast corner of the study area. Currently, there are no other interior roads. Two parcels located in the interior of the site do not have any road access.

All properties are currently served by individual wells and septic systems. This area does not have the water or sewer infrastructure for the type of job-creating businesses the City would like to encourage. Several owners have stated that if given the opportunity and infrastructure, they would like to upgrade their projects to uses that require infrastructure. Without the infrastructure, the land owners do not have many options for commercial development other than storage units.



Figure 2: Existing Site - 2004 Aerial Photograph and Parcel Map of the McCall Business Park site

CHARRETTE REPORT

OVERVIEW

The design charrette took place over a period of three days in McCall. Prior to the charrette sessions, Harmony Design & Engineering (HDE) and City officials contacted property owners, economic development specialists, City workers, public works experts, City committees, utility representatives and the general community to invite them to the charrette.

The participation in the McCall Business Park Charrette was outstanding. Over 53 stakeholders participated during the three day forum. A copy of the charrette schedule and a list of participants is included in Appendix A of this report.

MISSION STATEMENT

The charrette began with introductions of participants, a brief history of the project, and a discussion about the goals and mission of the charrette. The following mission statement was discussed and refined. This mission statement served to focus efforts and discussion toward the end goal.

Involve stakeholders and the public to develop a concept plan for a Business Park development in the Krahn Lane area that is compatible with the community's goals and integrates with the surrounding area.

The resulting Framework Plan will include proposed road systems, appropriate land uses and will outline general development standards. The plan will be used for further study and evaluation of infrastructure needs and costs.

COMMENTS FROM PARTICIPANTS

The first session began with stakeholders sharing their initial ideas and concerns about the project. Participants were asked to share their major concern and many comments were similar. Common themes included cost and funding of infrastructure, aesthetics of proposed developments, annexation requirements, and the effect on adjacent properties. Several residents expressed their concern about being forced to annex and connect to new city utilities. Neighboring residents were concerned about the impact of the Business Park development on adjacent residential properties. The following is a summary of the comments from the session. The original notes from this session are included in Appendix B.

- Interface/transition between the “urban” and “rural” parts of the project
- Sewer and water
- Shortage of shops
- Funding/cost of infrastructure
- Project funding
- Aesthetics and design standards (enhance McCall financially and visually)
- Annexation concerns
- Geotechnical analysis of site

- Stormwater runoff
- Economic development (grant opportunities)
- Appropriate scale included in the aesthetics
- This area is a Gateway to McCall
- Intersection of Krahn Lane and Hwy 55 issues
- “Business Park” ordinances need to be defined
- Integration of the Business Park into the area
- Continuity of uses
- Mobility
- Safety
- Integration of residential (walkability and connectivity)

A session was held to receive additional public input from neighbors and other community members. This Tuesday evening work session focused on ways to accommodate stakeholders concerns about the Business Park area. Participants were asked to write down their comments, concerns or desires and post them on a board under a specific category. After all the notes were posted, strategies for addressing those concerns were discussed and recorded. The original notes from this session are included in Appendix B. The following chart (Table 1) is a summary of the comments and associated strategies.

Table 1: Concerns and strategies generated during Tuesday's evening session

Category	Comment	Strategy
General Opportunities	Compliment existing businesses and services	Incorporate “goal statement” for businesses & green standards for businesses
	Should regulate use by noise and other environmental impacts, not type of business	Design ordinances to address the impacts of businesses- address noise, pollution etc. and set standards
General Concerns	Will sewer be able to accommodate future growth toward Timber Ridge	Sewer Master Plan needed
	Losing use of well	Annexation agreement could include provision to keep existing wells- possible to require change upon change of ownership (to be defined)
	Is there a mandated LID on the “Kings Site”	No mandated LID
	Development of non-sustainable businesses	Good economic analysis prior to construction
Land Use		Provide multiple/small lots
		Phasing
	Impact on existing residential & adjacent properties	Landscape buffers
		Land use gradation
	Compatibility with adjacent lands	Provide transitional areas
	Must have compatible use with surrounding properties	
	Yak Preservation	Krahn Lane as buffer/separator (between business park and agriculture)
	Small businesses	Plan for small business- scale buildings and lots
	Hours of operation	Buffer for light & noise
		Develop protective CC&Rs

Category	Comment	Strategy
Design Standards	Storm drainage- would like to have good, “green” storm water system that is well planned to handle existing & future flow that is coordinated with the surrounding systems	Mitigate stormwater using “green” technology (low impact development)
	Road structure- Creative, “green” streets approach (i.e. maybe not curb and gutter); snow plow friendly & snow storage friendly in aesthetically pleasing manner	Street section alternative to sidewalk, curb and gutter
	Structure density	Appropriate density in standards
	Shared parking/snow storage	Provide landscape buffers & open space that provides snow storage
	Landscape buffers	
	Maintain open space	
	CC&Rs	Enforce CCRs through business ownership association
	McCall Mountain Town architecture Metal Shops/buildings not desired	Architectural standards for mountain town- reference existing design standards or better
Economics	Green businesses	Provide incentives for green buildings and businesses
Transportation	Turn lane on Hwy 55/not a stop light	55 Intersection- add turn lane to accommodate traffic per ITD
	Turn lane traffic	
	Increased traffic & management of ingress/egress to arterial roads	Increase landscape buffers on Krahn Lane & Samson Trail
	Increase traffic (including large trucks)	Limit accesses to Samson Trail
	Concerned about traffic on Krahn & Samson- need to change speed limits	Upgrade street section on Krahn Lane & Samson Trail
	Promote <u>green</u> & walkable development with connectivity to other areas of McCall	Include pathways in plan
	Bike path	
	Pathway	
Details	Water system- well planned to provide looped system that is not piece-mealed together & provides adequate capacity	Provide looped water system in design
	Sewer- well thought out gravity lines to reduce chance of lift stations (best follow contours); need to coordinate these with planned road or utility ROW	Consider contours with road and sewer design as much as possible
	Water/sewer LID or possible annexation	Provide LID cost analysis in report
	Sewer extension cost	
	Cost of water/sewer LID	
	Utility Corridor (phone, power, cable)- Consider corridor for utilities that may not necessarily be in the road ROW, could be coordinated with snow storage & city utilities too.	Consider utility corridors- multi use and/or outside of ROW
	Define Business Park	Check DEQ requirements
	Krahn Lane & SH55 intersection- The Krahn Lift Station (LS14) sits on the NE corner of the intersection. Needs coordination with intersection improvements	Include a clear definition of Business Park in report Remember the lift station in design of intersection improvements

determined” use is an allowance for future uses that do not exist yet. For a complete list of brainstormed uses for the Business Park, see Appendix B. The following are the top ideas (those having 6 votes or more) identified. Each participant had approximately 10 votes.

- Web-based Businesses
- Software Development
- Indoor Recreation Center
- Research & Development Facility
- Vocational-Technical Center
- Sustainability Resource/Education Center
- Outdoor Gear Manufacturing
- Agricultural Uses
- Branch/Extension Education Facility
- Public Works Facility
- Government or Agency Offices
- Call Center
- Indoor Conference Center
- Yet to be determined

EXISTING INFRASTRUCTURE

A session regarding infrastructure was held to determine if there were any limitations on infrastructure development and how infrastructure might affect the Framework Plan for the Business Park. In attendance were representatives from the City of McCall (engineering, planning, and public works), telecommunication companies (Cable One and Frontier Communications), transportation organizations (ITD and Valley Transit), and the power company (Idaho Power). In a later session, input regarding waste management from Lake Shore Disposal and McCall Recycling Center was received.

In general, all entities agreed that this site could be served by adjacent facilities. However, utility mains will need to be extended and/or upgraded to provide service to the Business Park. The following are comments and recommendations made by each organization, generally discussing existing conditions and utility upgrades needed to serve a Business Park at Krahn Lane.

Power: Currently there is three phase power along Samson Trail and two-phase near the rest of the project that should be upgraded to three phase power. There are existing overhead power lines, but underground lines will be required for new development. Idaho Power would prefer an easement on private property to utilities located in public ROWs, and upgrade costs are born by the initiator of the upgrade. Idaho Power would need to put in transformers and the space needed would depend on the site. There is no natural gas system within the City of McCall at this time. See Figure 4 for existing dry utility layout.

Telecommunications: There is fiber optic cable on South Third Street/Hwy 55 from Frontier Communications and there is a fiber optic cable along Samson Trail from Cable One. There would need to be extensions from these locations into the Business Park area. Both companies can work in the “open

trench period” of Idaho Power and could share trenches. Both companies would encourage redundancy in the lines. Frontier Communications stated that they could install conduit during the open trench period and later pull the wire allowing for incremental growth. According to the Frontier representative, there might be funding options for these types of improvements in the second or third round of stimulus funding from the Federal Government. See Figure 4 for existing dry utility layout.



Figure 4: Existing Dry Utility Layout

Water: A minimum of 2,000 gallons per minute is required for fire protection. Construction of waterlines in Krahn Lane and Samson Trail will be required to create a looped system. Water mains will also need to be constructed along interior roads or within utility corridors throughout the project area. See Figure 5 for existing water system infrastructure.



Figure 5: Existing Water Line Layout

Sanitary Sewer: A sewer main extension will be required to serve the Business Park. Exact locations of sewer mains and the number and location of lift stations will depend on the configuration of the final plan. Currently there are no capacity issues for sewer service and the City believes a “will serve” letter could be issued. See Figure 6 for existing sewer infrastructure.



Figure 6: Existing Sanitary Sewer Layout

Waste Management: As the only current waste management providers, Lake Shore Disposal will serve the Business Park. They use 6' x 6' front load containers for commercial waste removal. These containers need to be in a corral and the corral (or enclosure) needs to be at the very least 10' x 10' (bigger is better) with good overhead clearance. It is also helpful if the corrals are angled to facilitate truck access. Lake Shore Disposal will also collect commercial cardboard in these same areas. Large compartmental containers need at least 60 feet of linear space and should be placed on concrete pads if possible. There also needs to be a snow removal ordinance in the CC&Rs or other code that ensures access to the corrals during winter.

Central recycling takes a lot of room and is expensive, but it would be useful to identify a spot for central recycling within the project.

Transportation: The intersection at Hwy 55 and Krahn Lane would need improvements to accommodate the Business Park. At a minimum, turn lanes on Hwy 55 from both the south and north will be required. Additionally, there is currently an existing building at the southeast corner of the intersection that is creating a site line issue that would need to be resolved. Snow storage and/or snow removal needs to be considered along streets and parking areas within the development. A transit stop within the Business Park should be provided and was encouraged by McCall Transit and Valley Connections.

DESIGN STANDARDS

Design elements such as street cross sections, architecture, landscaping, berms, buffers, alley access, lighting and setback requirements within the Business Park were discussed. Photographs of other Business Park developments in the west were presented and participants were asked to comment on the images. Most participants concurred that standards should be developed to assure aesthetic and functional quality of the Business Park. It was noted that this area is a “gateway to McCall” and its purpose is to attract and accommodate new businesses and industries. The following is a summary of comments made regarding design standards.

Street Cross Sections: Street cross sections for proposed interior streets and improvements to Krahn Lane and Samson Trail were discussed. The following illustration (Figure 7) was presented for comment. It was agreed that all streets should not have curb and gutter in order to facilitate snow removal and accommodate sheet drainage from paved areas to the 10’ wide vegetated bioswales on each side of the roadway. The bioswales will improve stormwater quality and provide an area for snow storage and infiltration. Street trees and sidewalks are desired along all interior streets within the Business Park. It was suggested that interior streets should have two 11’ travel lanes. Krahn Lane and Samson Trail would also need to be improved to meet City of McCall standards for collector streets with two 12’ travel lanes, a 2’ or 5’ gravel shoulder, and 10’ wide drainage swale. In addition, a landscape buffer and pathway easement should be provided on the Business Park side of both Krahn Lane and Samson Trail to create a connected pathway system that ties into existing or proposed pathways.

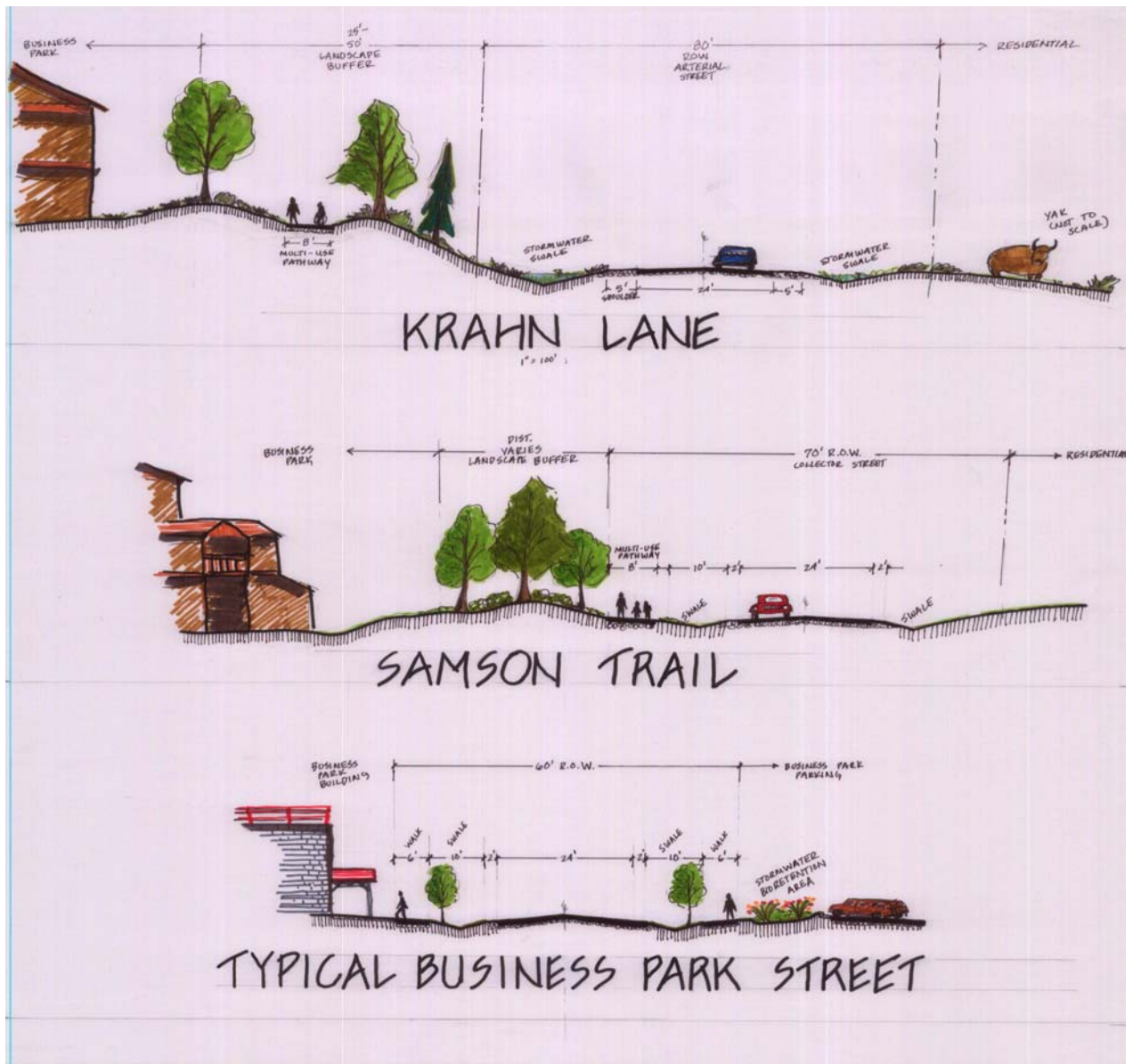


Figure 7: Street cross sections for Samson Trail, Krahn Lane, and a typical interior street

Landscape:

- Natural-looking berms
- Use of native and low-maintenance grasses and plants
- Use of plants that have some greenery in the winter (evergreens)
- Limit manicured, water-intensive plants to store fronts
- Preserve and use existing vegetation

Architecture:

- Diversity in design, materials and colors
- High quality

- Snow shedding consideration
- Different from downtown McCall architecture
- Materials reflect the service provided by the business
- Use of local materials
- Appropriate massing
- Articulation encouraged
- Metal buildings may be acceptable with appropriate architecture and aesthetic standards
- No massive, blank walls
- Incentives for LEED certification (or other green building standards)

Lighting:

- Standard lighting throughout the Business Park
- Unique lighting theme (i.e. “old-time industrial”)
- Low lighting at night
- Parking lot lights
- Use of solar and motion-detection lights encouraged
- Use of high-tech, low-energy use lights encouraged
- Lighting only at major intersections, minimal for function and safety
- Conform to dark sky code requirements

Site:

- Current ordinances for commercial zones in McCall are good
- Design review guidelines desired
- Increase setbacks adjacent to residential areas
- Provide lower maximum building heights adjacent to residential areas
- Set maximum building footprint standards, but have room for CUP approval if a particular business needs a larger building footprint

CONCEPT PLAN ALTERNATIVES

Using the background information gathered in earlier sessions, rough schematic and conceptual site plans were developed. Site plan options were discussed and charrette participants were encouraged to put their ideas to paper. Two general options were explored.

The first option was developed considering the entire Business Park area without the constraints of existing parcel boundaries. This plan incorporated a focal feature such as a park (or gathering place) in the center of the entire property. Higher intensity uses were focused in this area with intensity of use disseminating toward the edges of the property and toward residential areas. This plan had great potential for an interesting and creative design but would be difficult to implement due to numerous ownership of the existing parcels.

The second option used existing parcel boundaries as a framework for internal roads and organization. This plan seemed to have fewer options for creativity due to the constraints of the existing property lines, however it would be considerably easier to implement given the current multiple ownership structure.

Both plans had similarities in overall design concepts. These concepts included:

- High-intensity uses in the core of the Business Park
- Transitional zones to lower-intensity uses near surrounding residential areas
- Landscape buffers/pathways around the perimeter of the Business Park
- Hwy 55/Krahn Lane intersection upgrade
- Focal point in center of the Business Park

On the second day of the three day charrette, Harmony Design & Engineering refined the plans developed by the charrette participants as shown in Figures 8, 9, 10 & 11.

A schematic plan was developed that graphically depicted the general organization of the concepts and elements that were discussed in previous sessions (Figure 8). This plan shows locations of buffers and transition areas, connectivity of roads and pathways, and proposed improvements to existing streets and intersections. This schematic was used as a base to formulate the other concept plans.

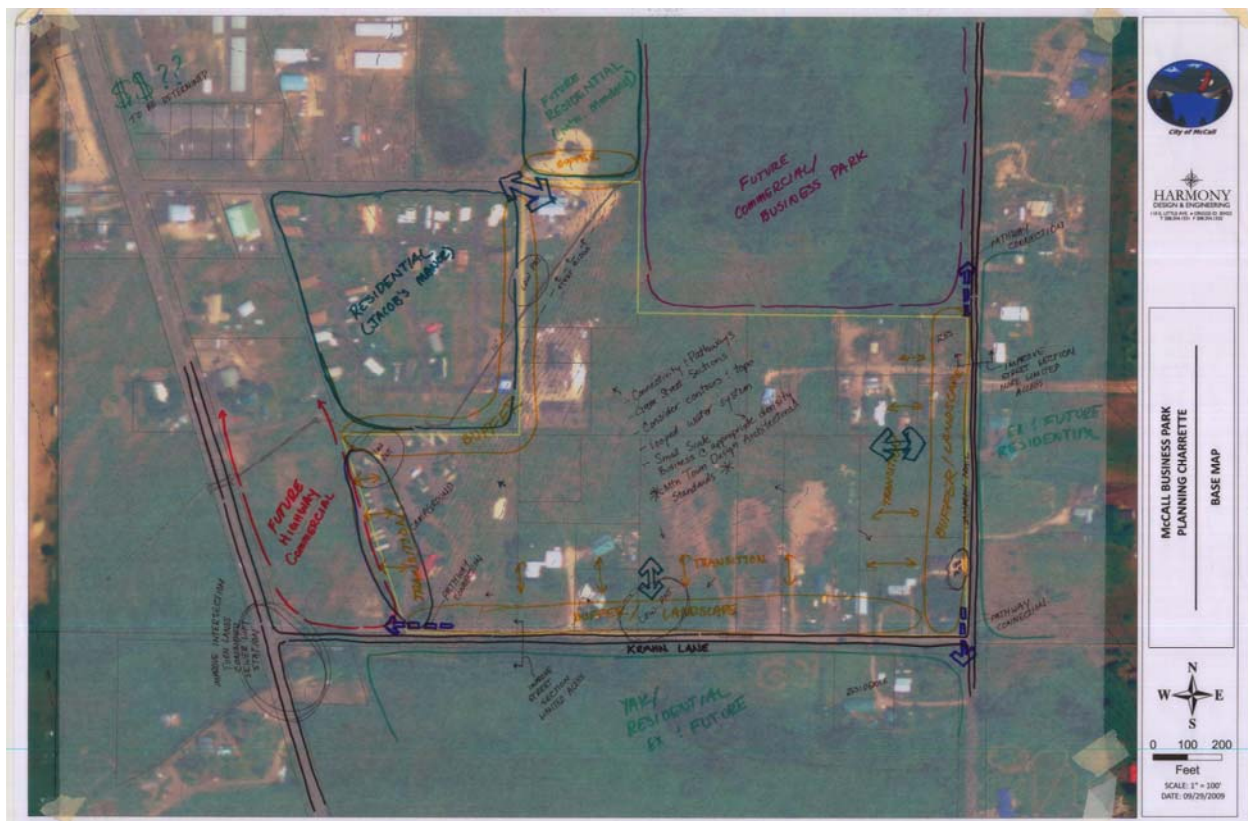


Figure 8: Schematic Plan, Graphical Representation of Desired Elements

Three concept plans, A, B & C were developed and presented to charrette participants theon day three of the charrette. Participants were asked to provide feedback on the concepts and evaluate each of the three plans by placing notes regarding what they thought was “GOOD” about each plan and what they thought could be “IMPROVED”.

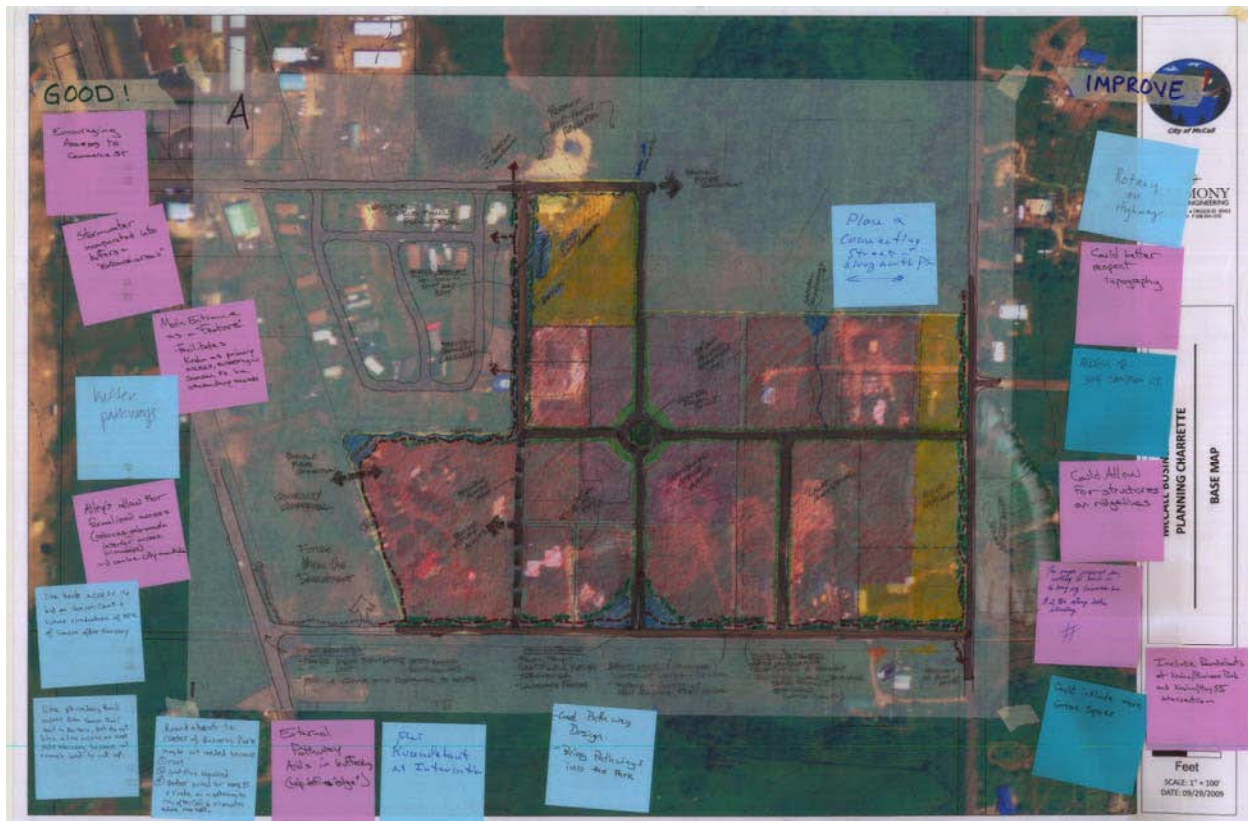


Figure 9: Concept Plan A- Streets follow property boundaries, existing uses not considered

Concept Plan A (Figure 9) included proposed interior streets located along existing parcel boundaries. Plan A shows existing non-conforming uses to be redeveloped with less intense uses adjacent to existing residential uses and more intense uses near the center. This plan also incorporates pathway connections, landscape buffers, and a “roundabout” at the main intersection.

“Good” comments include:

- Encourages access to Commerce Street
- Stormwater incorporated into buffers and entrance area
- Main entrance feature facilitates Krahn as primary access and Samson as secondary
- Good buffers and pathways
- Alleys allow for formalized access and reduces mis-mash driveways

“Improve” comments include:

- Plan a connecting street along north property line
- Eliminate roundabout and move it to the Krahn and Hwy 55 intersection
- Bring pathways into park
- Could respect topography better
- Needs access to 304 Samson Court
- Could allow for structures on ridgelines
- Could include more green space



Figure 10: Concept Plan B- Streets follow property boundaries, existing uses remain intact

Concept Plan B (Figure 10) included proposed interior streets located along existing parcel boundaries and allowed for existing residences and businesses to remain. This plan also incorporated pathway connections, landscape buffers, and a “green space” at the main intersection internal to the Business Park.

“Good” comments include:

- Accommodates existing and proposed uses
- Focal area respects topography (hill top)
- Gridded road system facilitates organized design
- Respects existing property boundaries

“Improve” comments include:

- Too many access points onto Krahn and Samson
- Does not look far enough into future
- Access to Samson Trail should be eliminated
- Needs better definition of edge of study area
- Needs street connectivity to adjacent property at north boundary and Samson Trail



Figure 9: Concept Plan C- Existing uses and property boundaries not considered, streets follow topography

Concept Plan C (Figure 11) did not consider existing parcel lines and the proposed interior street system generally followed the topography. This plan shows existing non-conforming uses to be redeveloped with less intense use adjacent to existing residential uses and more intense use near the center. The plan also incorporates pathway connections, landscape buffers, and stormwater facilities.

“Good” comments include:

- Topographically sensitive, curved roads follow contours
- Allows for greater added value
- Attractive to new businesses
- Good buffer zones for residents in surrounding land
- More cost effective

“Improve” comments include:

- Requires group collaboration, assumes combined ownership or arrangement
- Keep it simple (plan is too complicated)

Generally, participants liked the green buffers along the edges of the project that provide separation from adjacent residential uses and incorporate pedestrian and pathway connectivity. Plans with green spaces and interesting focal points within the site received favorable comments. Minimizing the number of access points to Samson Trail and Krahn Lane and providing alley access to parcels was preferred.

Plans that accommodated existing uses and considered existing property boundaries received mixed reviews. Some felt that a more interesting plan could be achieved if interior parcel lines were eliminated but many acknowledged the difficulty of coordinating a collaborative effort of all existing property owners.

There was significant discussion on the benefits and drawbacks of the two layout options, the first option not considering parcel boundaries as demonstrated in Plan C, the second option using the existing parcel boundaries as demonstrated in Plans A and B. The participants agreed that the logistical challenges posed by Plan C were too great to be realistically overcome. They also agreed that most of the good features of Plan C could also be realized and incorporated into Plans A & B.

PREFERRED PLAN

With the input of the previous three days and the evaluations of the Concept Plans A, B, & C, a preferred plan was formulated for further evaluation. The preferred plan was based on Plan A and incorporated elements from the Plans B & C that received positive feedback and considered the suggested improvements. To address what participants liked about Plan B, the Preferred Plan was created in two phases. Phase 1 (Figure 12) represents a short time frame when existing uses remain with a minimal amount of disturbance and infrastructure is constructed to allow improvements on currently undeveloped parcels. Phase 2 (Figure 13) represents a long term vision showing development potential on all lots. The framework of the interior streets and infrastructure is the same in both phases.

This preferred plan was presented at the final charrette session and one final round of feedback was solicited.

Much of the discussion of the final charrette session was focused on the next steps for the City and property owners. Next steps are presented in the following sections of this report.



Figure 12: Preferred Plan Phase 1 – short term (existing uses remain)



Figure 13: Preferred Plan Phase 2 – long term vision (existing uses not considered)

CONCLUSION

At the conclusion of the charrette the participants agreed that the preferred plan represented their goals for a Framework Plan for the Krahn Lane Business Park area. They felt it incorporated their concerns and worked within the reality of working with multiple land owners in the area. The participants felt the plan meets the goals of the mission statement by proposing a road system, appropriate land uses and general development standards that can be used for further study and evaluation of infrastructure needs and costs. The Framework Plan was developed by the community and integrates into the surrounding areas.

FRAMEWORK PLAN

PURPOSE OF THE FRAMEWORK PLAN

The purpose of the Framework Plan is to provide a framework for cohesive, efficient, and coordinated improvements in streets and infrastructure to allow for a variety of commercial and light industrial developments to occur. This plan was based on the preferred plan created through the design charrette process described in the previous section that considered input from all interested stakeholders to create a plan that the landowners can be vested in and is compatible with the community's goals.

This plan is a conceptual plan that is intended to serve as a guiding document for future development in the Krahn Lane / Samson Trail area. Actual locations of buildings, streets, secondary access ways, etc. can be adjusted provided the intent of the Framework Plan is met and adjacent properties are not adversely impacted.

Final development plans for specific properties within the Business Park are not indicated in the plan. Many options for size, location and configuration of buildings and other facilities as well as circulation patterns and further subdivision within the parcels are possible as long as it is compatible with the Framework Plan and meets the design guidelines. Master planning and re-platting of two or more contiguous lots is permitted and encouraged.



Figure 14: Conceptual Framework Plan for Business Park Area

ELEMENTS OF THE FRAMEWORK PLAN

The Conceptual Master plan (Figure 14) is a graphical representation of the concepts and configuration of elements discussed in the charrette.

Vehicular Circulation

Main roads within the Business Park generally follow existing property lines and would be located in a 60' right of way centered on the parcel boundaries. There are three accesses to Krahn Lane and one to Samson Trail. Street access is also provided to Jacob Street to the north. Potential future street connections are provided to the undeveloped property to the north. Secondary access alleys provide access to service areas and parking facilities at the rear of the lots. These secondary accesses should be in public right of ways as well and should allow for connectivity to adjacent properties. Parking lots and service areas are located at the rear or side of the lots. The "block" type street pattern could easily accommodate phasing of the Business Park. Intersection improvements at Highway 55 and Krahn Lane are shown in a roundabout configuration.

Pedestrian Circulation

Six foot wide detached sidewalks are provided on both sides of the all interior streets. Crosswalks are provided at all intersections. A 10' wide asphalt multi-use pathway is located along Krahn Lane and Samson Trail and is intended to connect to the City of McCall pathways network. Several locations near the main intersections are identified as ideal locations for pedestrian amenities such as plazas, small parks, or open spaces.

Street Frontage

Street trees are proposed in the vegetated strip between the road and the sidewalk. Proposed buildings are oriented to the street and are setback only a minimal distance. Pedestrian entrances are located on the street side of buildings.

Land Use

Developments of more intense use (shown in red in Figure 14) are located near the center of the Business Park. Properties near the perimeter of the Business Park and adjacent to existing residential zones (shown in yellow) are the least intense use. Landscape buffers are located along Krahn Lane and Samson trail to help mitigate the impact of the proposed development on adjacent properties.

Drainage / Water Quality Facilities

Storm runoff from the streets will be conveyed in vegetated bioswales located between the street and the detached sidewalk. Four detention / infiltration areas are shown throughout the Business Park at natural low points.

INFRASTRUCTURE REQUIREMENTS FOR FRAMEWORK PLAN

The Framework Plan illustrates how the proposed Krahn Lane Business Park may look when fully developed. Since development is likely to occur over an extended period of time (e.g., 10-20 years), understanding how the project is likely to be phased and the infrastructure requirements for each phase is essential. Until a more detailed project phasing becomes available, a general assessment of infrastructure requirements at full build-out of the Business Park is useful in determining what wet and dry utility services will ultimately be required. Accordingly, the following is a description of wet utility (water and sanitary sewer service) and dry utility (electric, gas, data/communications) that may be required to support a planned Business Park development.

General Assumptions for Estimating Infrastructure Needs: To develop infrastructure needs, the following assumptions were made:

Businesses - Based on the potential businesses identified in “POTENTIAL BUSINESS PARK USES” above, it was assumed that no significant water intensive or energy intensive processes would be used in estimating power and water needs. It is certainly possible that some business may become interested in the site that might require this assumption to change.

Floor Area Ratio - 0.27 (ratio of all floor space [includes levels over first floor] to total site space). The 0.27 ratio developed includes a 20% contingency. This covers the chance that 20% of the buildings could have a 2nd story.

Potential Number of Workers On-Site - Based on a compilation of averages from several sources, the calculations used in this document considered 1 worker per 150 square feet of building space. This number includes circulation and open or common space areas. At this rate, the site could support approximately 2,920 workers.

Water & Sewer Flow - Based on recommended water demand for an office/campus type setting (Metcalf & Eddy), this document uses a value of 16 gallons per day per worker. This is slightly above the average of 13 gallons per day, but was used to include a small percentage to cover minimal irrigation for the site. It is anticipated that green space will be accomplished with native and drought tolerant plantings. With 2,920 workers the average daily demand for water is approximately 46,720 gallons per day for the site.

Fire Flow - In order to test the water distribution system for a typical fireflow scenario, a 2,000 gallon per minute fire was modeled for a 2 hour period of time.

Dry Utility Location - All dry utilities will be installed underground and in a shared trench.

Power (Idaho Power Company): As noted previously, three phase power currently exists along Samson Trail and two-phase is adjacent to and serves the remainder of the existing project site. In order to properly serve the intended users, this estimate assumes that all site power should be upgraded to three phase power. Idaho Power has indicated that upgrade costs are borne by the initiator of the upgrade and so those are included in this estimate. At a minimum, Idaho Power would add another line to Krahn Lane, upgrading that system to 3 phase power. However, if any businesses have a larger than typical (office space) power need, it is possible that upgrades would be required at the transfer station. Other upgrades could include the installation of additional resistors, transformers and capacitors. The cost to bury power along Krahn Lane and through the site could require a price tag as high as \$1 million. However, this is a very high level estimate and should certainly be reviewed when more information is available. When more information is available, and for a fee, Idaho Power will develop an estimate. See Figure 15 for assumed location of future dry utilities.

As noted earlier, there is no natural gas system in the area which means that all power requirements would need to be supplied via electrical power or some other means that could become available (propane, wind, biofuels, etc.)



Figure 15: Proposed Location of Dry Utilities

Telecommunications: Fiber optic extensions from State Highway 55 (Frontier) and Samson Trail (Cable One) would be required to provide service to the interior core of the Business Park area.

Frontier Communications provided an estimate of \$48,000 for shared trenching and installation of conduit with fiber optic. The estimate does not include contingencies for such items as weather delays, rock excavation, or easement acquisition. According to the Frontier Communications representative, there might be funding options for these types of improvements in the second or third round of stimulus funding from the Federal Government.

Cable One provided an estimate of \$45,000 to relocate and bury their service lines and to extend service according to the Framework Plan. As with Frontier Communications, this assumes sharing a joint trench and does not include contingencies for weather delays, rock excavation, or easement acquisition. See Figure 15 for the proposed location of future dry utilities.

Water: A minimum of 2,000 gallons per minute (gpm) is required for fire protection; it is possible this could be as great as 3,500 gpm. However, for the purpose of this report, average or typical numbers are being used,

so as not to create an overly conservative estimate. Approximately 4,600 linear feet of waterline must be upgraded to a 14-inch diameter just to reach the northeast corner of the proposed site. Internal to the site, approximately 2,500 linear feet of 14-inch waterline is required along Samson Trail. An 8-inch watermain exists on State Highway 55 at the corner of Krahn Lane; according to results of the City's water model, this is sufficient at this time to serve the project, though the City's Water Master Plan indicates this will need to be upsized in the future. An additional 8-inch watermain is needed along Krahn Lane to create a loop between Samson Trail and State Highway 55. Multiple waterlines of various diameters are necessary to create an adequate network in the interior of the Business Park. See Figure 16 for proposed waterline layout.



Figure 16: Proposed Location of Water Utilities

At this time, the City and the Idaho Department of Environmental Quality (DEQ) have developed a letter agreement in which the City has consented to providing an updated Declining Balance Table whenever new water connections are made. Currently, the City's remaining actual capacity is adequate and a "Will Serve" letter could be provided. There are a number of significant water infrastructure improvements that are required by DEQ over the next several years. It is possible that this development would be asked to contribute to that work. The improvements would likely include supporting the design and construction of the 2nd water filter at the Water Treatment Plant, as well as potentially include installation of back up power systems at various water pump stations around the City. The cost could be as much as \$700,000. That number will not be included in the total estimated cost for water infrastructure at this point, as the certainty is very low at this time how those costs would be distributed.

The estimated cost for these improvements is just under \$1,100,000. This estimate includes watermain upgrades, new pipe for the interior, surface repair, fire hydrants and gate valves, and 3-valve clusters at intersections.

The cost estimate can be broken down into the following elements:

- Watermain Needs –
 - 7,100 linear feet of 14-inch watermain (4,600 linear feet to be upgraded and to reach the site plus 2,500 linear feet within the site) for approximately \$604,000
 - 1,000 linear feet of 12-inch watermain within the site for approximately \$60,000
 - 5,500 linear feet of 8-inch watermain including 2,100 linear feet along Krahn Lane and another 3,400 linear feet within the site for approximately \$220,000
 - Approximately 2,000 linear feet of 6-inch waterline serving hydrants or other minor spaces within the site for approximately \$60,000
 - All pipe costs include trenching, backfill, and surface repair. An additional cost of 20% is added to cover design.
- Fire Hydrants – Assumes 27 fire hydrants to cover the area for approximately \$97,000
- Gate valves to provide three valve clusters at each intersection for approximately \$48,000

Sanitary Sewer: Lift Station 14 exists at the corner of Krahn Lane and State Highway 55 and currently appears to have adequate capacity to serve this area. All flows would be conveyed generally from the northeast to the southwest to Lift Station 14. Long term plans for the City include eliminating Lift Station 14 and providing a gravity connection across State Highway 55 to the west and across the airport property. Regardless, site flows would be conveyed to the same location at the southwest corner of the proposed site.

Sewer main extension will be required to serve the Business Park. Exact concept locations of sewer mains and the number and location of lift stations will depend on the configuration of the final plan and development in the surrounding area; see Figure 17 for approximate locations used to develop this estimate. In addition to the gravity sewer lines networked through the interior of the proposed site, it is anticipated that a small lift station would be needed to boost sanitary sewer waste from the low spot on the site in the north east corner. Whether or not an additional lift station is needed will depend on more accurate topographical mapping and actual configuration of the site and the surrounding infrastructure at the time of development.



Figure 17: Proposed Location of Sanitary Sewer

At this time, the City is operating under a Consent Order with the Idaho Department of Environmental Quality (DEQ) and is required to provide an updated Declining Balance Table whenever new sewer connections are made. Currently, the City's remaining capacity is adequate and a "Will Serve" letter could be provided.

The total cost estimate for sanitary sewer improvements is approximately \$900,000 and can be broken down into the following elements:

- Small package Lift Station – Assumes 1,000 linear feet of 4-inch force main for \$375,000
- Gravity sanitary sewer – Assumes 8,000 linear feet of 8-inch pipe and 36 manholes. Cost includes trenching and surface repair for a total of \$525,000.

Storm Sewer: The preferred alternative made active use of the land to treat stormwater run off where possible by natural (vegetated swale) means. Detention ponds and swales have been incorporated into the development of the site as landscaping amenities. Based on a high level analysis of the stormwater volume developed as well the topographic situation at the site, it appears that four detention ponds would be required. Additionally, the stormwater system would include culverts for ditch crossings and revegetation.

The estimated cost to grade the ponds, excavate drainage swales, revegetate the site, and install culverts at access points, is about \$480,000. This does not include extensive landscaping. See Figure 18 for proposed storm sewer locations.



Figure 18: Proposed Location of Storm Sewer Utilities, Roads and Pathways

The cost estimate can be broken down into the following elements:

- Culverts – Assumes approximately 400 feet of 15-inch CMP for approximately \$22,000.
- Ponds – Assumes four detention ponds. Cost includes excavation and revegetation for approximately \$70,000 each or \$280,000 all together.
- Vegetated Swales – Assumes approximately 8,500 linear feet of vegetated drainage swale, including cost of excavation, grading, and general revegetation for approximately \$180,000.

Solid Waste Management: No further information is provided here for the solid waste management, as the infrastructure required to provide it is minimal and is tied very specifically to each individual site.

Transportation: This estimate assumes improvements to the intersection of State Highway 55 and Krahn Lane will be required. The preferred plan envisions a roundabout in place of a standard signalized intersection. A cost estimate is provided for a roundabout, but also includes an estimate for a more standard signalized intersection. The Idaho Transportation Department will play a significant role in determining what type of improvement will be made along the state highway at Krahn Lane.

Roundabout option – This option includes right of way acquisition, utility relocation, complete rebuild of the pavement structure into a roundabout, signing, and minor landscaping. If more elaborate landscaping is desired to create a gateway into the City, this would be an additional cost.

Standard intersection improvement – This option includes signalization of the intersection, widening of lanes on State Highway 55 to include a left turn lane from both the north and south directions (assuming there will be development to the west of the highway around the same timeframe), and widening of Krahn Lane at the intersection to include a right and left turn lane onto the highway.

The existing right of way width is currently unknown for State Highway 55 at the intersection of Krahn Lane. This information will be necessary to provide a more accurate estimate on the cost of the roundabout option. It will also be needed so that communication can be initiated with the property owners around the intersection. This analysis assumes that there currently is not adequate right of way for a roundabout.

Additionally, right of way for Krahn Lane is 60 feet and for Samson Trail is 50 feet. This is critical information to be able to provide adequate space for traffic circulation immediately around the proposed site. It is anticipated that semi-trucks and other large delivery vehicles will be a necessary part of the daily traffic at the site. With this in mind, care will need to be taken to ensure that turning radii are adequate to allow for these types of vehicles.

The cost estimate can be broken down into the following elements:

- Transit stop within the development – includes one stop with a shelter for about \$15,000.
- Concrete separated interior sidewalks (6-foot width) – Assumes approximately 8,500 linear feet of sidewalk throughout the interior. Cost includes excavation and grading for approximately \$400,000.
- Paved exterior pathway system along Krahn Lane and Samson Trail (10-foot width) – Assumes approximately 3,600 linear feet. Cost includes excavation and grading for approximately \$220,000.
- Paved interior streets (22-foot wide interior streets, no curb and gutter) – Assumes 4,250 linear feet of street, including excavation and grading for approximately \$780,000.
- Intersection Options:
 - Roundabout – This is a 4-legged roundabout with 2-lane roads approaching from all sides as noted above. Cost is approximately \$450,000.
 - Signalized Intersection – This option assumes a signal and widening of lanes as identified above for approximately \$350,000.

The total estimate for this component of the site is approximately \$1,900,000 for the roundabout option.

See Figure 18 for transportation elements.

Cost Estimate Summary:

The overall estimate of cost at this high level review of the project is as follows:

Utility	Low Range	High Range
Power		\$1,000,000
Telecommunications		
Frontier	\$48,000	\$58,000 ¹
Cable One	\$30,000	\$45,000
Water	\$1,100,000	\$1,80,000 ²
Sanitary Sewer	\$900,000	\$1,250,000 ³
Storm Sewer	\$480,000	
Transportation	\$1,800,000	\$1,900,000 ⁴
Solid Waste	N/A	N/A
TOTAL	\$4,358,000	\$6,053,000

Each Estimate Contains a 20% Design Component.

¹ Contains a 20% contingency for rocky soils, weather delays, and easement issues.

² Includes cost of system-wide infrastructure improvement (specifically a portion of the cost of construction of the required additional water filter).

³ Includes a second lift station.

⁴ Includes the cost of the roundabout. (the low cost estimate includes intersection improvements and signalization).

NEXT STEPS

The following are plans and studies recommended to further refine this Business Park report and determine further details. Once the Land Use Feasibility Analysis, the Detailed Phasing Plan, Utility Study and the Traffic Study are completed, the Framework Plan could be fine-tuned to reflect the new information developed in these studies.

Land Use Feasibility Analysis

This analysis would confirm the viability and sustainability of the Krahm Lane Business Park. The Land Use Feasibility Analysis would include a demographic review of the community to better understand the local population, workforce and real estate trends and a review of current absorption rates for existing Community Commercial and Industrial land uses.

Detailed Phasing Plan

A more detailed phasing plan would allow for phased construction of utilities. More accurate cost estimating could be developed based on this information. Infrastructure improvements could then be adopted into the City's Capital Improvement Program.

Traffic Study

This study would research information on right of way, develop potential traffic volume requirements and detailed information on needed improvements. This would be developed along with the phasing plan.

Phased Utility Plan

This study would quantify water and wastewater requirements over a phased period based on the detailed Phasing Plan identified above.

CITY STAFF ACTIONS

The following are next steps for City Staff to complete to further the process:

1. Staff develops an email distribution list and mailing for all landowners and participants. The lists will be used to communicate meeting dates and notify participants regarding the report (completed).
2. City staff and consultants present the report to the City Council and the Council adopts the report as a planning document.
3. City Council directs staff to amend the Future Land Use Plan (FLUP) to include the area plan for the Business Park location. Staff starts the process to amend the FLUP.
4. Staff creates Business Park Zone ordinance based on the report, other standards and review of other communities' codes. Staff presents the new Business Park Zone ordinance to the McCall Area Planning and Zoning who makes recommendation to the McCall City Council.

5. The City of McCall acts as a resource to help landowners or investors navigate steps of the process.

LANDOWNER ACTIONS

The following are next steps for the property owners within the Business Park study area:

1. Property owners review report and provide public comment to the City of McCall.
2. Landowners organize meeting to determine how they would like to proceed with moving the Business Park plan forward.
3. Once a decision has been made, organized landowners contact the City of McCall on what actions they would like to see happen.
4. If the landowners want to proceed with the plan, additional plans and studies should be pursued to further define the feasibility and phasing of the project. Depending on the funding and timing of the project, an annexation plan needs to be developed.

ANNEXATION OPPORTUNITIES

The timing of annexation will need to be determined based on the on the overall plan to implement the project which will include property owner participation, funding sources, and the findings of the market study and phasing plan. Depending on the overall strategy, there could be positives and negatives of annexation based on the timing of annexation.

According to the McCall City code, prior to annexation of an unincorporated area, the Council shall request and receive a recommendation from the Planning and Zoning Commission respecting the potential zoning of the unincorporated area. Both the Commission and the Council shall follow the notice and hearing procedures provided in Idaho Code section 67-6509 for hearing the issue of zoning upon annexation. Concurrently, or immediately following the adoption of annexation, the Council shall amend as necessary the comprehensive plan and zoning map.

The following is a summary from Idaho Statute 50-222:

“cities of the state should be able to annex lands which are reasonably necessary to assure the orderly development of Idaho's cities in order to allow efficient and economically viable provision of tax-supported and fee-supported municipal services, to enable the orderly development of private lands which benefit from the cost-effective availability of municipal services in urbanizing areas and to equitably allocate the costs of public services in management of development on the urban fringe.”

There are three categories of annexation within Idaho Code. Only 2 of the categories may apply to the Business Park study area and are described below:

Category A Annexation

- All private landowners have consented to annexation.

- Any residential enclaved lands of less than one hundred (100) privately-owned parcels, irrespective of surface area, which are surrounded on all sides by land within a city or which are bounded on all sides by lands within a city and by the boundary of the city's area of impact; or
- The lands are those for which owner approval must be given as described below in *Evidence of consent to annexation*.

Upon determining that a proposed annexation meets the requirements of category A, a city may initiate the planning and zoning procedures to establish the comprehensive planning policies, where necessary, and zoning classification of the lands to be annexed.

Category B Annexations

- The subject lands contain less than one hundred (100) separate private ownerships and platted lots of record and where not all such landowners have consented to annexation; or
- The City may annex lands that would qualify under the requirements of category B annexation if the following requirements are met:
 - The lands are contiguous or adjacent to the city and lie within the city's area of city impact;
 - The land is laid off into lots or blocks containing not more than 5 acres of land each, whether the same shall have been or shall be laid off, subdivided or platted in accordance with any statute of this state or otherwise, or whenever the owner or proprietor or any person by or with his authority has sold or begun to sell off such contiguous or adjacent lands by metes and bounds in tracts not exceeding 5 acres, or whenever the land is surrounded by the city. Splits of ownership which occurred prior to January 1, 1975, and which were the result of placement of public utilities, public roads or highways, or railroad lines through the property shall not be considered as evidence of an intent to develop such land and shall not be sufficient evidence that the land has been laid off or subdivided in lots or blocks. A single sale after January 1, 1975, of 5 acres or less to a family member of the owner for the purpose of constructing a residence shall not constitute a sale.

Evidence of consent to annexation

Consent to annex is valid only when evidenced by written instrument executed by the owner or the owner's authorized agent. Consent shall be implied for the area of all lands connected to a water or wastewater collection system operated by the city only if the connection was requested or completed prior to July 1, 2008. Written consent to annex lands must be recorded in the county recorder's office to be binding upon subsequent purchasers, heirs, or assigns of lands addressed in the consent. Lands need not be contiguous or adjacent to the city limits at the time the landowner consents to annexation for the property to be subject to a valid consent to annex; provided however, no annexation of lands shall occur, irrespective of consent, until such land becomes contiguous or adjacent to such city.

Preparation and publication of a written annexation plan

Preparation and publication of a written annexation plan, appropriate to the scale of the annexation contemplated includes, at a minimum, the following elements:

- (A) The manner of providing tax-supported municipal services to the lands proposed to be annexed;
- (B) The changes in taxation and other costs, using examples, which would result if the subject lands were to be annexed;
- (C) The means of providing fee-supported municipal services, if any, to the lands proposed to be annexed;
- (D) A brief analysis of the potential effects of annexation upon other units of local government which currently provide tax-supported or fee-supported services to the lands proposed to be annexed; and
- (E) The proposed future land use plan and zoning designation of designations, subject to public hearing, for the lands proposed to be annexed;

Public Notice

The initial notice of public hearing concerning the question of annexation and zoning shall be published in the Star News and mailed to every property owner with lands included in such annexation proposal not less than twenty-eight (28) days prior to the initial public hearing. All public hearing notices shall a 1 page summary of the contents of the city's proposed annexation plan and shall provide information regarding where the annexation plan may be obtained.

FUNDING OPPORTUNITIES

The following are some of the opportunities that currently exist that could potentially provide for infrastructure improvements in the project area. Funding levels and availability change over time, so no guarantee of funding is provided through this summary.

FEDERAL

The Economic Development Administration (EDA) has several grant programs that could apply to the proposed project area. It must be noted that EDA programs typically are reserved for areas that are economically distressed. The current economic conditions in Valley County would most likely qualify the area as economically distressed, as the unemployment rate for December, 2009 is 19.3%.

Public Works and Economic Development Program: Public Works and Economic Development investments help support the construction or rehabilitation of essential public infrastructure and facilities necessary to generate or retain private sector jobs and investments, attract private sector capital, and promote regional competitiveness, including investments that expand and upgrade infrastructure to attract new industry, support technology-led development, redevelop brownfield sites and provide eco-industrial development. (www.eda.gov)

Economic Adjustment Assistance Program: The Economic Adjustment Assistance Program provides a wide range of technical, planning and infrastructure assistance in regions experiencing adverse economic changes that may occur suddenly or over time. This program is designed to respond flexibly to pressing economic recovery issues and is well suited to help address challenges faced by U.S. regions and communities. (www.eda.gov)

STATE

Idaho Community Development Block Grants: The Idaho Department of Commerce administers federal funding from Housing and Urban Development (HUD) to address a variety of community needs. Idaho Community Development Block Grants are available to cities and counties and can be used for situations of imminent threat, public facilities and affordable housing, senior and community centers, and economic development activities such as downtown revitalization and job creation. This could be a potential source of funding for infrastructure improvements in the study area, if the proposed improvements are to alleviate “slum and blight” in the project area or if jobs will be created resulting from infrastructure improvement for a specific business expansion or relocation. Grant awards for economic development activities can range up to \$500,000. (www.commerce.idaho.gov)

Idaho Gem Grants: The Idaho Department of Commerce administers state funding for the Idaho Gem Grant program. Job creation and retention are the primary objectives of the IGG program. Grants up to \$50,000 may be awarded to eligible communities for the purpose of developing job creating assets. Some examples of successful IGG projects include water and sewer infrastructure for new a business, remediation

of slum and blight conditions blocking business development, and matching funds for the creation of assets with a high certainty of aiding future economic development efforts. (www.commerce.idaho.gov)

LOCAL

Local Option Tax: The City of McCall administers a local option tax for short term rentals. This generates approximately \$300,000 each year, and is subject to the occupancy of hotels, motels, and other short term rentals. Applications for the funding are reviewed and prioritized by an independent citizen committee, with the final decision made by the McCall City Council. Paving, pathways, and parks are some of eligible activities for this funding. (www.mccall.id.us)

Local Improvement District: The Idaho State Legislature has provided that municipalities may establish Local Improvement Districts for the purpose of constructing and financing the cost and expense of improvements to the exterior portions of business buildings in order to bring business buildings within such districts into conformity with an architectural theme adopted by the city. Property owners must agree to participate in the LID in order to be taxed for the improvements.

Tax Increment Financing or Urban Renewal District: Tax Increment Financing – the financial method used in an Urban Renewal District - is a public financing method that is used for redevelopment and community improvement projects. Urban Renewal Districts have been formed to address both blight and economic development issues. Generally, the idea behind this type of financing is that a specific District is created in which property taxes are frozen at the time of the creation of the District. This becomes the baseline of taxes in which the tax increment will be placed. The goal is that the District can use future gains in taxes (property, sales and use taxes) to finance current improvements (which theoretically will create the conditions for those future gains). For example, when public improvements (water, sewer, roads, etc.) are constructed, the surrounding real estate typically sees an increase in value. This increase in value generates the increased property tax revenues. The construction of the improvements, created additional use tax revenue and the opening of commercial development will ultimately increase sales tax revenues over and above the baseline taxes.

The District can borrow money based on the increased tax revenue that will be coming directly to them as an agency. The standard method for borrowing money is through a bond. In Idaho, an Urban Renewal District is a separate entity from the City and does not need voter approval to borrow money. There are other methods that have been used with regard to borrowing as well. Some Districts have created an Owner-Partner Agreement whereby the City and a developer create an agreement in which the City borrows from the developer. In this case, the City then pays back the developer over time, similar to paying back a bond. The developer is then collecting money both on the sale of real estate in the District as well as from the City in payment for the borrowed money for the infrastructure.

CODE RECOMMENDATIONS AND DESIGN GUIDELINES

PURPOSE OF THE BUSINESS PARK ZONE

The McCall Area Comprehensive Plan has an economic development objective to “encourage diversification of the economy by providing zoned and serviced land for Business Parks and light industry.” It is the purpose of this document to implement the plan’s vision through development regulations that allow for these types of uses, and create functional, aesthetic, high quality Business Parks to attract businesses that can provide stable, high paying jobs for the McCall community. These regulations will also serve to protect resources, enhance property values, integrate with the surrounding areas and land uses and employ sustainable development practices.

The following sections provide recommended language for inclusion in the new Business Park Zoning Code. These recommendations are derived from suggestions from the community, property owners, city staff and other stakeholders during the Business Park design charrette. Existing land use regulations in other chapters of the McCall planning and zoning regulations, and representative land use codes from similar zones in other communities in the intermountain west were also used to determine these uses.

RECOMMENDED SUBZONES

During the design charrette, it was determined that the impacts of Business Park Zone activities should be minimized where the zone abuts an existing residential zone. Therefore, two sub-zones within the Business Park Zone are recommended. The BP-1 zone is located within the first 200 feet of the perimeter of the Business Park property where it abuts an existing residential zone (along Krahn Lane and Samson Trail). The purpose of this sub-zone is to limit uses to those that will have limited impact on adjacent properties. The more intense uses that are likely to have greater impacts of noise, vehicular traffic, and activity shall be located in the BP-2 sub-zone which is located on the interior portion of the site and along boundaries adjacent to properties with similar permitted uses. The framework Zoning plan (Figure 19) shows the limits of these sub-zones.

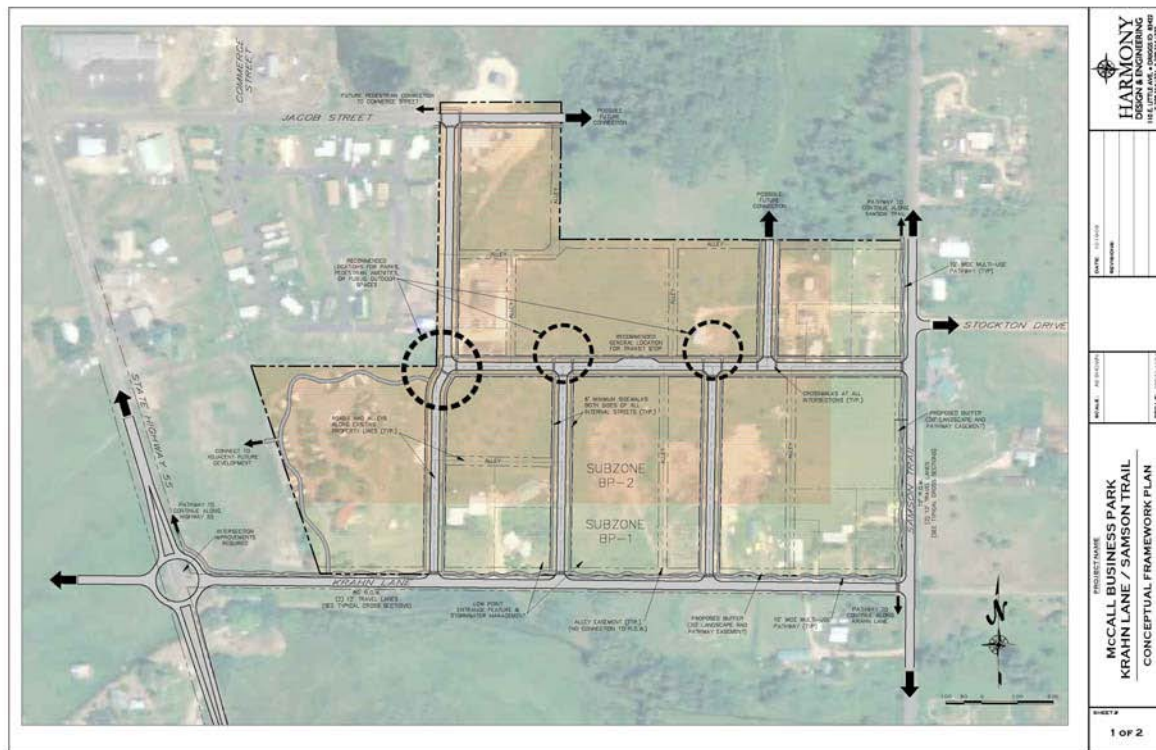


Figure 19: Conceptual Framework Plan with Zones BP-1 and BP-2 Delineated

BUSINESS PARK USE REGULATIONS

The intent of this section is to:

- Allow a wide variety of uses that are compatible with each other
- Attract high quality businesses and industries
- Discourage uses that are better served in other downtown commercial or industrial zones
- Minimize impacts on adjacent residential zones

The uses identified in the following chart represent recommendations for permitted uses in the Business Park Zone.

P – Permitted Use, C – Conditional Use, N – Not Permitted

USE	BP-1	BP-2
Administrative offices	P	P
Amusement or recreation facility (indoor)	C	P
Amusement or recreation facility (outdoor)	N	C
Animal clinic, animal hospital, or veterinary office	C	C
Automobile, major repair	N	C
Bank	C	P
Bar, brewpub	N	C
Business support services	C	P
Call center	C	P

Clinic, medical	P	P
Computer, software development, web based business	P	P
Contractor's yard or shop	C	C
Convenience store	C	C
Convention facility, conference center	C	P
Dwelling, single family detached	N	N
Dwelling, attached (see note 1)	P	C
Government or agency offices	P	P
Industrial, heavy	N	N
Industrial, light	C	C
Mixed use	C	C
Nursery, wholesale	C	C
Nursery, retail	C	C
Manufacturing, light	N	C
Manufacturing, heavy	N	N
Package and letter delivery service	P	P
Professional office	P	P
Public or quasi-public use	P	P
Public parks, plazas, recreation areas or easements, trails, pathways	P	P
Public service facility	P	P
Radio and television broadcasting station	C	P
Recycling center (serving Business Park only)	N	C
Research and development facility	C	P
Restaurant	C	C
Retail stores, when ancillary to a primary use (see note 2)	P	P
Retail business, large scale	N	C
School, vocational technical or trade	C	P
School, environmental resource center	C	P
School, university extension	C	P
Service retail business	P	P
Storage facility, self service	C	C
Storage facility, warehouse	C	C
Storage facility, outdoor (see note 3)	N	C
Studio (music, art, dance or similar studio)	P	P
Tower or antenna structure	N	C
Transit facility (bus stop, shelter)	P	P
Warehouse and wholesaling facilities	N	C
All other uses not specified	C	C

Notes:

1. Dwelling units must be part of a mixed-use project and only permitted on upper floors. The maximum allowed density is 40 units per acre.
2. Ancillary uses are secondary uses and activities that are associated with and support the primary use. They typically require a minor portion of the square footage of the space of the primary use.
3. All outdoor storage areas shall be completely screened from view from any adjacent street.

RECOMMENDED BUSINESS PARK GENERAL DEVELOPMENT STANDARDS

Except as modified below, all development shall comply with the McCall City Code, Title 3, Chapter 8, General Development Standards.

Dimensional Standards: The standards in the following chart are recommendations for dimensional standards in the Business Park Zone. These recommendations were derived from suggestions from the community, property owners, city staff and other stakeholders during the Business Park design charrette. Existing land use regulations in other chapters of the McCall planning and zoning regulations, and representative land use codes from similar zone designations in similar communities were also used to determine these standards.

In general, these recommendations are a combination of McCall's existing community commercial and industrial standards. During the Business Park design charrette, it was generally agreed that there was no need to drastically diverge from McCall's existing standards for these zones although there was some concerns about building heights adjacent to the surrounding residential areas. Therefore, building heights are recommended to be 35' or less within the BP-1 subzone to help assure that proposed structures are in scale with adjacent residential properties.

The intent of this section is to:

- Allow adequate space for vehicle and pedestrian circulation
- Allow compact development and efficient use of site
- Protect solar access to adjacent properties
- Orient buildings and pedestrian entrances to the street
- Provide a transitional buffer from adjacent residential zones
- Provide flexibility to allow for innovative and creative design solutions
- Assure that proposed developments are in scale with the area

Dimensional Standards by Business Park Zone

Minimum property size	4,000 square feet
Maximum structure height	35' (up to 50' in BP-2 only, with CUP) (see note 1)
Maximum building footprint, each building	25,000 square feet (up to 40,000 in BP-2 only, with CUP) (see notes 1 & 2)
Maximum lot coverage of impervious surfaces	80%
Minimum street frontage	40'
Minimum setback from:	
Street, frontage	10' (could be 0' with CUP) (see note 3)
Abutting other zones	25'
Property line not fronting a roadway	10' (or 0' for attached structures)
Minimum distance between buildings	15' (or 0' for attached structures)
Maximum setback (build-to line)	15' in BP-2, none in BP-1 (see note 4)

Notes:

1. Building height and footprint allowances in the BP-2 subzone may be increased to the maximum indicated with the issuance of a conditional use permit. Approval of the CUP will be considered if the proposed development provides significant, attractive and functional civic space for public use and/or provides superior architectural design elements such as those recommended by LEED or

another design standard. Buildings greater than 35' are subject to "step back" requirements per Chapter 8 "General Development Standards".

2. Individual lots may have multiple buildings in a "campus" configuration.
3. The minimum front setback may be 0' for pedestrian oriented architectural features such as a store front, vestibule, open-air lobby, etc.
4. At least one primary building entrance or other pedestrian building element shall be built within fifteen feet (15') of the front property line in the BP-2 subzone. An exception to this build-to line may be granted through design review when the project contains pedestrian amenities between the primary building entrance and the front property line. Pedestrian amenities including parks and plazas are allowed and encouraged.

Design Review / Design Guidelines: Design review is required for all developments within the Business Park Zone as provided for in Chapter 16 of the McCall city code. Design review is a process whereby the City of McCall incorporates the "City Of McCall Design Guidelines" into new construction and remodeling of structures, landscaping, lighting, and public amenities within the city and the Area of Impact. The "City Of McCall Design Guidelines" is a separate document that is incorporated into this chapter by reference.

The intent of the Business Park Zone is to provide ideal areas to attract and accommodate new, high-quality, stable businesses and light industries that will foster economic growth and diversify the economy of McCall. Therefore, it is critical that all development within this zone be held to a level of quality that enhances the area as a whole and creates an attractive and desirable environment within the Business Park.

Public Improvements (Streets, Sidewalks, Pathways, Utilities, Etc.): Except as modified below, all public improvements including streets, sidewalks, pathways, and utilities shall be in accordance with the McCall City Code, Title 9, Chapter 6, Subdivision and Development Improvement Requirements.

All streets and secondary access ways within the Business Park Zone shall be constructed per City standards and shall be located in a platted public right of way. A minimum 6' wide concrete sidewalk is required on both sides of all interior streets where shown on the Framework Plan. Secondary access ways and alleys may have sidewalks where appropriate however they are not required. A minimum 10' wide asphalt multi-use pathway is required around the perimeter of the Business Park where shown on the Framework Plan. Crosswalks shall be provided at all intersections for all sidewalk and pathway crossings. Crosswalks may be painted or constructed of an alternate paving material such as concrete or precast pavers.

The following figures are typical cross sections of all improved streets within and adjacent to the Business Park Zone. These street cross sections are designed to provide:

- Sufficient travel way for passenger cars and truck traffic
- Adequate space for snow storage
- Street side swales for drainage, infiltration and stormwater quality
- An attractive streetscape with landscaping to enhance proposed buildings
- Sidewalks and pathways to accommodate and encourage pedestrian circulation
- Buffers and screening for adjacent residential zones

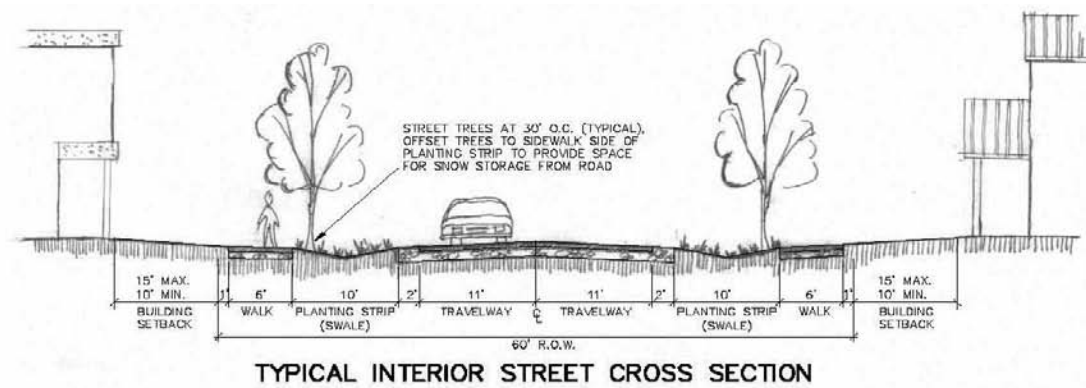


Figure 20: Typical Interior Street Cross Section



Figure 21: Samson Trail Cross Section

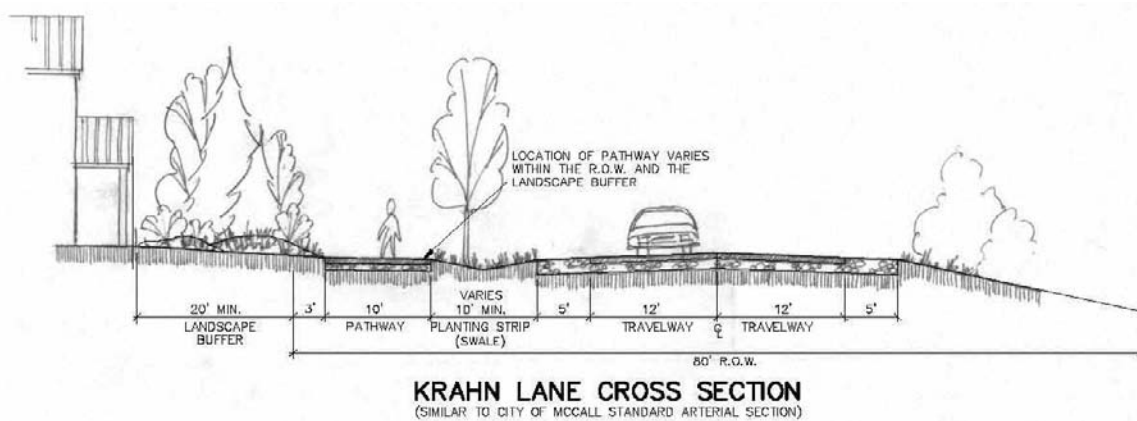


Figure 22: Krahn Lane Cross Section

Hours of operation / noise restrictions: Hours of operation of businesses within the Business Park are not restricted except that any activities that produce plainly audible noise in excess of the following levels shall not be permitted. The intent of this requirement is to:

- Reduce the impact of Business Park activities on surrounding residential areas
- Reduce the impact of the Business Park on other businesses and live work units within the Business Park

No business activities shall produce regular or continuous noise that is excessively annoying, loud, or unusual. The maximum allowable noise levels as measured on any adjacent receiving property shall be:

60 dBA for daytime hours (8:00 AM to 9:00 PM)

55 dBA for nighttime hours (9:00 PM to 8:00 AM)

Noise that is produced for no more than a cumulative period of 5 minutes in any daylight hour may exceed the standards above by 10 dBA.

Planned Unit Development Requirement: Planned unit developments are permitted in the Business Park Zone and can be applied to individual lots or a contiguous area consisting of multiple lots. Any proposed planned unit development shall be in accordance with the McCall City Code, Title 3, Chapter 10, Planned Unit Developments.

Vehicle and Bicycle Parking: Parking for vehicles and bicycles shall be provided in accordance with the McCall City Code, Title 3, Chapter 8, General Development Standards.

Architectural: All developments within the Business Park Zone shall comply with the “City of McCall Design Guidelines” with regard to architectural improvements. Design review is required for all proposed structures within the Business Park Zone. McCall City Code Title 3, Chapter 16, Design Review should be modified to include Business Park Zone.

Landscaping: Except as modified below, all developments within the Business Park Zone shall comply with the “City of McCall Design Guidelines” with regard to landscape improvements and irrigation systems. The purpose of this section is to:

- Reduce water use requirements on the property
- Increase the visual interest and natural look of the Business Park
- Provide a buffer between the Business Park and the surrounding residential areas
- Encourage the use of native species
- Provide infiltration capacity for the Business Park

A landscaped perimeter buffer strip and pathway shall be provided along Krahn Lane and Samson Trail as indicated on the Framework Plan. Plant materials shall be native or adaptive species with low water requirements. The overall design shall mimic the natural landscape of the region. A variety of plant materials that include deciduous and evergreen trees and shrubs as well as native grasses and groundcovers shall be used to create an interesting and effective screen of the proposed development. Plant densities shall be sufficient to provide an effective screen within a period of five years. In general, the plant densities shall be 10 shrubs and 3 trees per 1000 square feet of landscape area. Plant quantities can be reduced if larger specimens are provided. Native boulders can be added sparingly if they are placed such that they become an integral feature in the naturalistic landscape. The construction of earthen berms is encouraged to add interest

and additional buffering from adjacent developments. Berms shall be irregularly shaped into natural forms of varying height and width. Continuous berms of uniform height and width are not permitted. Landscaping shall be designed to blend with adjacent properties to result in a harmonious corridor. Buffer strip landscaping need not be a continuous solid screen. Gaps and openings may be designed to add interest and highlight or frame interesting architectural features or modest signage. Openings may be provided and are encouraged to allow pedestrian accesses from the pathway to pedestrian building entrances adjacent to the buffer. The following illustration shows a typical landscape buffer as viewed from Krahn Lane or Samson Trail.

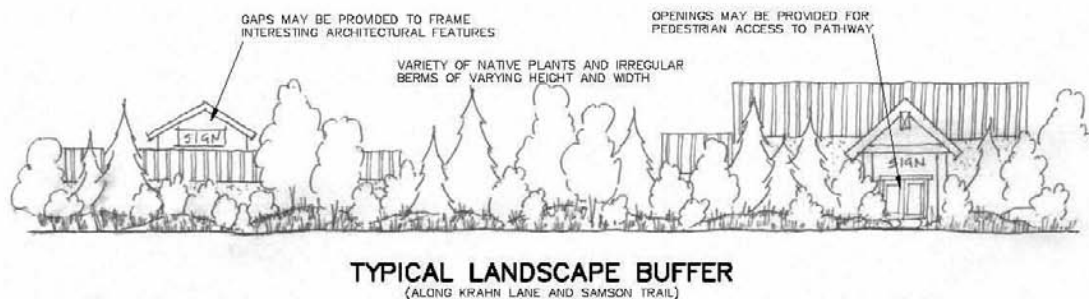


Figure 23: Typical Landscape Buffer

Street trees and native grasses shall be provided in the swales between the edge of pavement and the sidewalk along all interior public streets. Street trees shall be planted a minimum of 30 feet on center along the entire length of the street. Exceptions to this standard can be considered to accommodate innovative and interesting landscape design alternatives at pedestrian amenities, building entrances, plazas, and parks. Street tree species shall be in accordance with the City of McCall approved species list.

Irrigation systems shall be provided for all new landscapes. The systems should be continuously maintained in a functional condition. Low water consumption irrigation systems are encouraged. Irrigation systems for native landscapes and xeriscapes may be turned off if plants are well established and remain in a healthy and attractive condition without supplemental water.

Fencing: All fencing within the Business Park Zone shall be in accordance with the McCall City Code, Title 3, Chapter 8, General development Standards and the “City of McCall Design Guidelines”.

Lighting: Except as modified below, all lighting within the Business Park Zone shall be in accordance with the McCall City Code, Title 3, Chapter 14, Outdoor Lighting and the “City of McCall Design Guidelines”.

In general, site and parking lot lighting should be kept to the minimum required for function and safety. Street lighting shall be kept to a minimum and provided only at main intersections and pedestrian gathering areas such as transit stops and plazas. Consideration should also be given to the design of interior lighting so as not to produce offensive glare on adjacent exterior spaces, especially along public streets.

Sustainable Construction Standards: Projects shall be built to sustainable construction standards. These may be defined by Resolution.

Possible Incentives for Sustainable Development:

- Reduced parking requirements for bicycle facilities and showers and transit stop
- Reduced parking requirements for designated carpool only parking spaces.

- Increase in allowable impervious lot coverage for green, sustainable practices that reduce stormwater runoff (i.e. rainwater re-use, rain gardens, green roofs).
- Reduced building permit fees for facilities and practices that significantly reduce water consumption thus reducing the demand on city services.
- Allow height and area bonuses for superior architecture, for providing extra civic space, recycling facility space and service or a transit stop, or for master planning two or more contiguous lots.

APPENDIX A: CHARRETTE SCHEDULE AND PARTICIPANT LIST

CHARRETTE SCHEDULE

Tuesday (09/29) Session One at LEGION HALL: Site Evaluation and Program Development

HDE = Harmony Design & Engineering

Time	Task	Objectives/Outcomes	Participants
9:00	Project overview, introduction, discussion of goals	Project orientation, align goals	HDE, Property Owners, City Officials
10:00	Discuss land ownership, replatting options, common space, marketable lot sizes	Background information	HDE, Property Owners
11:00	Discussion of types of uses and economic feasibility of uses	Background information	HDE, City Officials, Chamber of Commerce
12:00	LUNCH		
1:00	Refine site analysis	Background information	GIS Analyst, HDE, City Engineering Staff
1:30	Discuss infrastructure needs: utilities, water, sewer, electric, phone	Background information	City Engineering Staff, HDE
2:00	Discuss connectivity of roads, pathways, open space	Background information	City Engineering Staff, HDE
3:00	Conceptual plan sketches	Start developing Framework Plan	HDE, Public, City Officials
5:00	BREAK		
5:30	Start Public Workshop- overview of charrette process, discuss goals, desired uses, property rights	Gain public input on desires, goals and visions	All Stakeholders and Public, HDE
6:00	Break into work groups: Public draws ideas and gives input	Gain visual understanding of public desires	All Stakeholders and Public, HDE
7:30	ADJOURN		

Wednesday – Harmony Design Work Day / Open Workshop 9:00 – 6:00 at the PUBLIC LIBRARY

This day will be used to organize the information accumulated from Design Charrette One. We will refine the concept plan and prepare exhibits of results from day one. Harmony staff will be available to receive additional public input and meet with other interested parties. The workshop will be **open to the public but will not have a structured agenda.**

Thursday (10/01) Session Two at LEGION HALL: Public Evaluation and Site Plan Refinement

Time	Task	Objectives/Outcomes	Participants
9:00	Review of concept plan exhibits	Get feedback	City Officials, HDE, Land Owners
10:00	Study and discussion of typical site details	Feedback, Design Development	HDE, City Officials
10:30	Examine street dimensions, parking requirements, service access	Feedback, Design Development	HDE, Fire officials, City Engineers, City Planners
11:00	Design standards (architectural, landscaping, lighting)	Feedback, Design Development	HDE, City Planners, Chamber Members
12:00	LUNCH		
1:00	Recycling/waste management	Feedback, Design Development	HDE, City Engineers
2:00	Refine Concept Plans	Create visual representation preferred plan	HDE
5:00	BREAK		
5:30	Public Workshop: Describe process and current plans	Clarification for Public/HDE	All Stakeholders and Public, HDE
6:00	Public input on conceptual plans (dot exercise)	Feedback	All Stakeholders and Public, HDE
6:45	Discuss next steps	Invite continued feedback	All Stakeholders and Public, HDE
7:30	ADJOURN		

PARTICIPANT LIST

Date	Name
29-Sep	Mr. & Mrs. Bruce , Property Owner
29-Sep	Kelly Clark , Property Owner
29-Sep	John Blaye , Economic Development
29-Sep	Ronda Sandmeyer , Citizen, MAMA
29-Sep	Robert Lyons , Citizen, MAMA
29-Sep	Kim Apperson , P&Z Commissioner, Neighbor
29-Sep	Kim Kirkland , Property Owner
29-Sep	Robin Lester , Property Owner
29-Sep	Jim Lester , Property Owner
29-Sep	Regina Ventress , Property Owner
29-Sep	Bret Huggins , Property Owner
29-Sep	Laura Crawford , Property Owner
29-Sep	Dave Peugh , Land Planner
29-Sep	Andy Laidlaw , Local Architect
29-Sep	Phil Choate , ITD
29-Sep	Randy Geddes , Property Owner
29-Sep	James E Betts , P&Z Commissioner
29-Sep	Michelle Groenevelt , City of McCall Community Development Director
29-Sep	Carol Coyle , City of McCall Grant Coordinator
29-Sep	Natalie Spencer , P&Z Commissioner
29-Sep	Tim Cochrane , Chamber of Commerce Director
29-Sep	Jay Mentzer , Treasure Valley Transit
29-Sep	Steve Williams , Citizen
29-Sep	Rick Kittrell , Property Owner
29-Sep	Mark Wood , Idaho Power
29-Sep	Betsy Roberts , CH2MHill, City Engineer
29-Sep	Levi Brinkley , City of McCall Water/Sewer Superintendent
29-Sep	Rick Scherette , City of McCall
29-Sep	Josh Kriz , Central District Health
29-Sep	John Lewinski , City of McCall Water/Sewer Plant Superintendent
29-Sep	Shorty Clarke , Chair, Park & Rec Advisory Committee
29-Sep	Richard Jayo , Frontier Communications
29-Sep	Roger Snodgrass , Citizen
29-Sep	Joe Baugh , Property Owner
30-Sep	Dale Tomevi , Property Owner
30-Sep	Dave Peugh , Epikos Land Planner

Date	Name
30-Sep	Mary Hart , Recycle Partners
30-Sep	Anna Clark , Property Owner
30-Sep	Rick Kittrell , Property Owner
30-Sep	Betsy Roberts , City of McCall Engineer
30-Sep	Michelle Groenevelt , CD Director
1-Oct	James E Betts , P&Z Commissioner
1-Oct	Ronda Sandmeyer , Citizen, MAMA
1-Oct	Robin Lester , Property Owner
1-Oct	Jim Lester , Property Owner
1-Oct	Bradley Kraushaar , City of McCall Planner
1-Oct	Rick Kittrell , Property Owner
1-Oct	John Blaye , Economic Development
1-Oct	Kim Kirkland , Property Owner
1-Oct	Michelle Groenevelt , CD Director
1-Oct	Don Bailey , City Council
1-Oct	Becky Johnstone , Citizen, Realtor
1-Oct	Mike Medberry , Citizen, Idaho Solar Power
1-Oct	Bob Burnop , Property Owner
1-Oct	J. Summers , Police Chief
1-Oct	Randy Geddes , Property Owner
1-Oct	Mike Anderson , McCall Real Estate
1-Oct	Sarah Jessup , P&Z Commissioner
1-Oct	Natalie Spencer , P&Z Commissioner
1-Oct	Jay Mentzer , Treasure Valley Transit
1-Oct	Regina Ventress , Property Owner
1-Oct	Bret Huggins , Property Owner
1-Oct	Dan Gallagher , Long Valley Advocate
1-Oct	Thomas Frasier , Lake Shore Disposal
1-Oct	Everette Arter , Lake Shore Disposal
1-Oct	Carol Coyle , Grant Coordinator
1-Oct	Brian O'Morrow , Brown's Industries
1-Oct	Steve Jones , Easter Creek
1-Oct	Laura Crawford , Property Owner
1-Oct	Brad Bever , Property Owner
1-Oct	Karla Stevenson , Citizen

APPENDIX B: CHARRETTE EXHIBITS

COMMENTS AND CONCERNS

Major Concerns

- Interface/transition b/t "urban & rural"
- Sewer/Water
- Shortage of shops
- Funding/Cost of Infrastructure
- Project Funding
- Aesthetics/Design Standards!
 - enhance (financially & visually) M'Call
- Annexation concerns

Major Concerns

- Costs/Aesthetics
- Water/Sewer
- Infrastructure
 - Geotechnical Analysis
 - Stormwater Runoff
- Economic Development
 - Grant opportunities
- Appropriate Scale/Aesthetics
 - GATEWAY to M'Call (prec)
- Intersection Krahn/Hwy 55
- "Business Park" Ordinances
- Integration of Business Park

Major Concerns

- Aesthetics
- Continuity of uses
- Mobility & Safety
- Integration of residential
 - walkability
 - connectivity

Overall Goals

- Connectivity
- Aesthetics
- Infrastructure (& Funding)

POTENTIAL LAND USES

Types of Uses

- ★ ★ Makes a product for export
 - i.e. freeze-proof irrigation
 - computer software development (office space)
- ★ ★ - Promote home-business ventures ★ ★
 - (light industrial - low-impact)
- Services
- Agricultural ★ ★ ★
- Nuclear
- ★ Medical
- Branch/Extension Education

- ★ ★ Call Center
- ★ ★ ★ R & D (chem, bio, high-tech...) ★ ★
- ★ ★ Nursing outreach
 - backcountry health care
- Flight school
 - ★ ★ helicopter
- Yet to be determined ★ ★ ★

- ★ ★ Vo-Tech Ctr ★
- ★ ★ Business Incubator
- ★ ★ "Green"/Sustainability Education/Resource Ctr.
- ★ ★ Guiding Services
- ★ ★ Outdoor gear manufacturing
- ★ ★ NOLS-type school
- ★ Hospitality school
- Ceramics - Conference/Events Ctr.
- Quilts
- Insulated Shades
- Banners/Seamstress business

- ★ ★ playground / rec equipment manufacture
- ★ ★ dry cleaning
- ★ ★ health care
- Solar panel manufacture
- ★ ★ retail outlet for solar
- ★ ★ ★ ★ web based businesses ★ ★ ★
- nursery / landscape ★ ★
- ★ ★ electronic, medical
- ★ ★ nano technology
- ★ ★ gym
- ★ ★ indoor rec / climbing wall

POTENTIAL LAND USES, CONTINUED

musical instrument manufac*

wood working tools*

biomass chipping

Public works facility*****

fisheries *

gov't / agency offices*****

**utilities offices

***Small start up businesses*

sewing, bakers

artists lofts** *

*leather manufacturing

*life style business *

Software development

-shops/residential

-respect property boundaries

-draft a "dreamy" plan

-Don't limit potential future uses

INFRASTRUCTURE

Power: ^(Doodler)

- 3-phase on East (Samson)
- 1-phase on Krahn (upgrade for \$)
- prefer 20' easement (can do 10')

can share
trenches

Telecommunications: ^(Doodler)

- Fiber Optic on 3rd (would need spur)
- Need locations & densities
- could put in conduit & pull wire later

Water: ^(Doodler)

- Fire protection = big requirements
- Need loop

Sewer: ^(Doodler)

- Krahn Ln. lift str. is "temporary"
- no capacity issues at this time

CONCERNS AND STRATEGIES

Q

Incorporate "goal statement" for businesses ; green standards ; businesses

Mon?

Impacts of businesses - address noise, pollution, etc ; set standards

GENERAL

OPPORTUNITIES

Landscape buffers

Land use gradation

Provide transitional areas

Krahn Lane as buffer/seperator? - attractive to Yaks

Plan for small business - scale bldgs ; lots

Buffer for light ; noise

CCRs

IMPACT OF EXISTING RESIDENTIAL PROPERTIES

PROPOSED CHANGES TO EXISTING PROPERTIES

PROPOSED CHANGES TO EXISTING PROPERTIES

PROPOSED CHANGES TO EXISTING PROPERTIES

PROPOSED CHANGES TO EXISTING PROPERTIES

PROPOSED CHANGES TO EXISTING PROPERTIES

LAND USE

No mandated L.I.D.

Sewer master plan needed

Provide multiple / small lots

Good economic analysis prior to construction

Phasing

Annexation agreement includes provision to keep existing wells

possible require change upon ownership change

PROPOSED CHANGES TO EXISTING PROPERTIES

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PROPOSED CHANGES TO EXISTING PROPERTIES

PROPOSED CHANGES TO EXISTING PROPERTIES

GENERAL CONCERNS

Architectural standards for mtr

Town - Reference ex. Design Standards or better

Provide landscape buffers ; open space that provides snow storage

Enforcement of CCRs

Appropriate density -

Mitigate stormwater using "green" technology

Street section alternative

PROPOSED CHANGES TO EXISTING PROPERTIES

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PROPOSED CHANGES TO EXISTING PROPERTIES

DESIGN STANDARDS

CONCERNS AND STRATEGIES, CONTINUED

Incentives for green bldg
?

ECONOMICS

55 Intersection - add turn lane to accommodate traffic per ITD

Upgrade street section Kraln; Sampson

Include pathway ~~is~~ in plan

Increase landscape buffers

55 Intersection - add turn lane to accommodate traffic per ITD

Upgrade street section Kraln; Sampson

Include pathway ~~is~~ in plan

Increase landscape buffers

55 Intersection - add turn lane to accommodate traffic per ITD

Upgrade street section Kraln; Sampson

Include pathway ~~is~~ in plan

Increase landscape buffers

TRANSPORTATION

Remember lift station

Include a clear definition of Business Park in Report

LID & analysis in Report

Consider utility corridors - multi use? outside of ROW.

Consider contours w/ Road; sewer design as much as possible

Looped water system

Check DEQ requirements

DETAILS

55 Intersection - add turn lane to accommodate traffic per ITD

Upgrade street section Kraln; Sampson

Include pathway ~~is~~ in plan

Increase landscape buffers

55 Intersection - add turn lane to accommodate traffic per ITD

Upgrade street section Kraln; Sampson

Include pathway ~~is~~ in plan

Increase landscape buffers

APPENDIX C: ENLARGED FIGURES FROM REPORT